

Nancy L. Mello
Town Clerk

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TOWN COUNCIL /JOINT WORKSHOP WITH PLANNING BOARD

In accordance with the Open Meetings Law, Section 42-46-6, notice is hereby given that the Town Council will hold a Workshop on Monday, January 30, 2012 at the Tiverton Town Hall, 343 Highland Road at 7:00 p.m.

JOINT WORKSHOP AGENDA

1. Call to Order
2. Wind Turbine/Industrial Park and Other Areas of Tiverton
Presentations
 - a. Apex Wind
 - b. East Bay Energy Consortium
 - c. Town Planner – DiPrete Engineering Development Plans – Report of Infrastructure Installation Costs
3. Joint Discussion with Planning Board Regarding Planning Board Activities, Priorities and Areas of Focus

A portion of this meeting may be held in executive session pursuant to the Open Meeting Law, Section 42-46-5(a) of the General Laws of Rhode Island, 1956 (1986 Reenactment), as amended.

Nancy L. Mello
Town Clerk

TOWN OF TIVERTON, RHODE ISLAND

TOWN COUNCIL

January 12, 2012

Ladies and Gentlemen:

You are receiving this communication and copy of a November 28, 2011 presentation to the Tiverton Town Council because you may have some potential interest in the Town of Tiverton Industrial Park. Please note that the term "stakeholder" as used in the presentation document merely indicates that the people or organizations listed may have some interest in future development at the Tiverton Industrial Park. The term does not signify nor imply that a particular person or organization has been an active participant in work related to the industrial park.

The Town Council has scheduled a work shop on January 30, 2012 at 7 PM in Town Hall to hear presentations from the East Bay Energy Consortium and Apex Wind Energy about possible installations of wind turbines at the Tiverton Industrial Park. Other pertinent information may also be presented. Please plan on attending if you have any interest in the possible future of wind turbines located in or near the Tiverton Industrial Park.

To date the Tiverton Town Council has not taken any votes concerning an installation of wind turbines at the Tiverton Industrial Park.

In a related, but separate matter there is a development plan, including some consideration for wind turbines, for the industrial park making its way through the Town planning process. The proposal, which has been created by a consulting engineering firm paid by grant funding, has been granted preliminary master plan approval and is waiting for further work by the consulting engineers before any further Town Planning Board action is taken.

For the Tiverton Town Council,

Cecil E. Leonard, Vice President

Cc: Stakeholders

BRIEFING PAPER FOR THE TIVERTON TOWN COUNCIL

November 28, 2011

Presented by Councilor Cecil Leonard for discussion at 12/12/11 meeting.

OBJECTIVE:

Gain the maximum return on the industrial park as measured by monetary return, aesthetics' that will maintain or enhance the "character" of the Town and creation of a "foundation" for the future Tiverton.

Given the above metrics the question is what are the best uses for the industrial park? This question segues to the question of what is the relative competitive position (RCP) of the park in relation the other industrial sites in Rhode Island and nearby Massachusetts?

Detailed information is not available at this time, but some macro factors can be noted. First by dint of its location in the anti-business (see a variety published sources) and nearly bankrupted Rhode Island it is at a disadvantage. Second, because it does not have infrastructure in place and has a significant amount of rock ledge within its boundaries it is at a further disadvantage at this time.

The second infrastructure factor is being addressed to some degree by industrial park development plans currently in process.

ADDITIONAL ISSUES TO BE CONSIDERED:

Wind turbine development in Tiverton has been a matter of public discourse for several years, with various related activities ongoing and various land owners involved. It would be prudent, therefore, to review this complex matter and communicate the Town's position to all stakeholders.

Issue: Status of wind energy project deliberations at the industrial park.

Comment: No decision has been made; Tiverton is one of 9 towns cooperating on the East Bay Energy Consortium (EBEC) study of the idea of a municipal cooperative development; a private developer is also interested in the Tiverton site. Obviously, the benefits of wind energy development must be weighed against benefits and potential of other developments on the site.

Issue: Why strong interest in Tiverton for wind energy development?

Comment: The industrial park along with several surrounding parcels is a highly desirable site (undeveloped, relatively isolated, large area, close to grid for connection, windy). This has resulted in the EBEC study and a number of private developers proposing projects on water authority (fire districts) and other private properties.

Issue: Town moratorium on wind turbine development.

Comment: The moratorium is in effect, which prevents developments until the adoption of a Tiverton wind turbine ordinance.

Issue: Wind turbine ordinance.

Comment: To be drafted by the Planning Board, this is pending issuance of state sitting guidelines. The state guidelines are probably 3 months away; a local ordinance then probably needs 3 months to draft, hold hearings, and approve. Ordinance will address small, land owner turbines as well as a large, utility scale project in the industrial zone in the vicinity of the industrial park.

Issue: Potential for different property owners combining for a large "wind farm."

Comment: The Town industrial park, water authority (fire districts) properties, and several private parcels could potentially be used for a single large wind energy project employing several turbines.

Issue: Planning coordination involved for a wind farm involving multiple landowners.

Comment: The wind turbine ordinance should consider the need for a master plan for wind energy development in the Industrial Zoned area to ensure coordinated planning, assess overall visual impact, and design grid connectivity. (Because of the large expense and complex approval process involved with grid connection, the RIPUC would likely require this).

Issue: Wind Energy Development on parcels surrounding the industrial park if the Town decides not to approve turbines at the park.

Comment: A number of parcels outside the park might be suitable for private, utility scale wind energy development, but that must wait until the moratorium is lifted, and a multi-turbine design would have to comply with whatever provisions are written in the new ordinance which may have a master plan component.

Issue: Current development proposals.

Comment: The Town is aware of two current interests, the EBEC study and Apex Wind, Inc is considering the Tiverton industrial zoned area. The Council may hear presentations from both in January.

Issue: Potential cost to the Town to develop turbines at the industrial park.

Comment: No finished wind energy development proposals have been offered, so there have been no financing plans considered. An entering assumption, however, is that the Town will most likely not approve a project to be financed in a way that increases Tiverton's property tax rate.

Issue: Cost of installing sewer, electrical and telephone/data lines to industrial park lots.

Comment: An estimate of such infrastructure costs to make lots "pad ready" have been requested from the engineering firm doing the industrial park development proposal

Issue: Decision process.

Comment: There are at least three major decision points:

Decision 1 – Whether to incorporate wind energy within the industrial park.

Decision 2 – If the decision is to develop wind energy, then whether to join a municipal coop or "go it alone."

Decision 3 – If Tiverton goes alone with a private developer, some form of open, competitive bidding process would be used to choose the developer.

STAKEHOLDERS:

Town of Tiverton property tax payers, Town Council, Planning Board, Wastewater Management Commission, Conservation Commission, Open Spaces and Land Preservation Commission and Economic Development Commission.

North Tiverton Fire District
Stonebridge Fire District

East Bay Energy Consortium
Apex Wind, Inc. and/or other private wind energy enterprises

RI Economic Development Corporation
Gerald Felise, President, eCo Industrial Park of Rhode Island
Douglas Industries, Inc.



TIVERTON PLANNING DEPARTMENT

346 Judson Street • Tiverton, Rhode Island 02878
(401) 816-0494

TIVERTON INDUSTRIAL / TECHNOLOGY/ENTERPRISE PARK DEVELOPMENT OVERVIEW AND STATUS

Background

In 1996 the Town of Tiverton purchased approximately 200 acres of land north of Fish Road and east of Route 24 for the creation of an industrial/ technology park. The principal and interest on the twelve (12) year bond totaled approximately \$5.4 million dollars. The land was purchased from the Tiverton Industrial and Recreational Commission which had purchased it for \$1.5 million dollars in 1988. Other than the creation of a 27.77 acre lot that was sold to Tiverton Power Associates, and the construction of Progress Way, the park remains essentially unimproved and has not been subdivided into smaller marketable parcels.

Over the years, a number of small and large scale development proposals have been advanced, but none have materialized for one reason or another. A number of factors that may be attributed to the lack of progress with development of the park are:

- The park's topography and soil conditions contribute to very high site construction costs for large-scale developments;
- The park has never been subdivided, so smaller sized parcel have not been available for sale or development;
- The park's infrastructure has not been fully developed or constructed;
- The overlying zoning is not conducive to development—as an example, even industrial or manufacturing uses are not allowed by right in an industrial zone;
- No master plan for the park existed; and
- Development plan review for the park was not a straightforward process, and no mechanism existed for expedited review.

Given the above roadblocks to development, the Planning Department has been working to create a subdivision plan for the park, improve the zoning, develop a set a Standards and Guidelines, and create an expedited review process for future development.

Working with the Newport County Chamber of Commerce, and with the assistance of a \$60,000 grant from the U.S. Small Business Administration, the Planning Department crafted a Request for Proposals (RFP) that would direct the engineers to develop a plan that would include a “mix of parcels sizes to accommodate a broad range of development interests, take advantage of the topography, site constraints, existing infrastructure, minimize additional infrastructure costs, and facilitate future parcels mergers for businesses anticipating future expansion.”

The subdivision plan, proposed zoning changes, and the Tiverton Enterprise Park Development Standards and Guidelines have been developed to be consistent with Tiverton's Comprehensive Community Plan which outlines the following goal, objectives and policy:

Goal:

Expand economic development opportunities in Tiverton, and pursue and support forms of development that will maximize economic benefit while preserving and enhancing the environment and character of the town.

Objectives:

- Ensure that the local zoning regulation accurately reflect the goals and desires as stated in the Comprehensive Community Plan for future commercial and industrial development, particularly in terms of location a categories and intensities of use;
- Plan for and support only commercial and industrial development that provides net tax benefit and is at a scale and design that is consistent with the town; and
- Maximize economic opportunities in the town industrial / technology park.

Policy:

Promote the development of the industrial park for suitable light manufacturing, technology, office and associated uses.

Project Status

DiPrete Engineering has completed the Concept Plan Review and Master Plan Review with the Planning Board where they were granted Conditional Master Plan Approval for the subdivision. Currently they are working on the engineering and roadway and drainage structures for submission to the Rhode Island Department of Environmental Management (RIDEM).

The next steps in the review process are the Preliminary and Final Plan review by the Planning Board. Upon Final Plan Approval, DiPrete will submit final plans for recording of the Industrial Park Subdivision.

The Planning Board is also currently reviewing a proposed Floating Zone, entitled Planned Development Park (PDP) which will be applied to the Industrial Park and will expand the allowed uses and overall development potential. The PDP also incorporates specific signage, site, landscaping, lighting and design regulations. In addition, the Planning Board is also reviewing the proposed Tiverton Enterprise Park Development Standards and Guidelines for the park. Together, these two documents provide for and expedited review of future development proposals.

Tax Projections

The following *conservative* estimate of the taxes that the Industrial Park would generate at full build-out were provided by Tiverton Tax Assessor, David Robert. They are based on concept of 81 lots, but assume mergers of lots (for a total of 70) with an average lot size of 2.25 acres.

Sale of Lots: 70 Sites @ \$125,000 = \$8,750,000 in sales to the Town. (One time income).

Assessment of Lots:

70 Sites @ \$125,000 = \$8,750,000 X current tax rate of \$15.71 = \$137,500

Assessment of Buildings:

850,000 Square Feet @ \$135 per Square Foot = \$114,750,000 X current tax rate of \$15.71 = \$1,802,700

The value per square foot will vary depending on the structure type. Storage/Light Industrial type buildings will yield a lower PSF, while Office/Medical would assess out significantly higher. A blended rate is used.

Tangible Personal Property:

Tiverton's current ratio between TPP and the commercial assessments is 20%. However, a significant portion of our TPP assessments are for utility assets such as Narragansett Electric and Cox Cable. Therefore, using a ratio of 15% seems more appropriate.

Commercial Valuation of Industrial Park = $\$123,500,000 \times 15\% = \$18,525,000 \times \$15.71 = \$291,000$

Summary:

When adding all of the above components together, the annual taxes assessed would be \$2,231,200. Adding the total value to our current tax base would have offset the current FY tax rate by \$0.94, reducing the average taxpayer's tax bill by \$290.

Please note that this is a cursory review provided by the Tax Assessor, who was asked to provide a conservative estimate of what could be expected on an annual basis once the Industrial Park is fully built out.

Additional Revenue

In addition to the annual taxes that would be collected, as well as the one time sale of lots, other revenue that will be provided at a future date, include:

- Plumbing Permits
- Mechanical Permits
- Electrical Permits
- Building Permits
- Fire Inspection Permits
- Sewer Fees

Employment Projections

Due to the variety of permitted uses, it is difficult to estimate employment within the park at this time. However, DiPrete Engineering, in their estimation of sewer flows has provided an estimate of 6,583 employees which is based on a useable building area of 987,500 square feet and assumes one (1) employee for every 1,5000 square feet of floor area.

In addition, there would be a number of construction jobs, engineering, and architecture jobs associated with the park build-out as well as a multiplier effect for the local economy.

Infrastructure Cost Estimates

Infrastructure cost estimates, which will be provided by DiPrete Engineering as part of their contract, are not available at this time. They will be provided after the engineering is further along.

Paying for the Infrastructure

Both the infrastructure estimated costs and regional economic benefit will be utilized to complete a Community Economic Development Strategy (CEDS) application to Statewide Planning. The project must be placed on the Rhode Island's CEDS Priority list to be eligible for U. S. Economic Development Administration (USEDA) funding for infrastructure improvements.

CODE OF ORDINANCES
TOWN OF
TIVERTON, RHODE ISLAND
APPENDIX A
ZONING

ARTICLE IV. DISTRICT USE REGULATIONS

Section 19. Planned development park

A. Purpose. It has been determined that:

1. There is a need for a planned development park(s) in the Town of Tiverton which may emphasize industrial, technology, scientific, medical, renewable energy, commerce or office uses; and
2. That such uses are in accordance with the Tiverton Comprehensive Community Plan. The purpose of this section is to provide for planned development parks in appropriate locations and settings.

B. Definitions.

1. *Planned Development Park:* (PDP): A development for the uses hereinafter enumerated in this Section, subject to the applicable dimensional provisions of this zoning ordinance.

This definition of PDP shall be considered to be a definition as if it were a part of a Article II – Definitions; of this zoning ordinance.

2. *Monument Sign:* A type of freestanding sign with a permanent base affixed to the ground which is at least two-thirds the horizontal length of the sign.
3. *Attached Sign:* A type of sign attached to a building, either parallel or perpendicular to the plane of the exterior wall.

C. Requirements.

1. A PDP shall have both public water and access to public sewerage, nearby highway access and shall be constructed on a lot or combination of contiguous lots (“contiguous” lots shall mean abutting lots or lots divided by an easement or right-of-way, natural barrier, or any private or public street other than a state highway) containing not less than 150 acres of land, which may be further subdivided in accordance with the provisions of this zoning ordinance and Tiverton’s Land Development and Subdivision Regulations.
2. *Development Standards and Guidelines.* A PDP must have an adopted set of Development Standards and Guidelines which are specific to the park.

- D. Zoning District.* A PDP shall be allowable in an Industrial (I) zone under this zoning ordinance, subject to the specific area requirements as set forth above and to the further requirements of this section.

E. *Permitted uses in a PDP.* The following uses are permitted in a PDP:

1. Motel or hotel (with restaurant, including entertainment and/or liquor).
2. Professional office and bank, credit union or office building as permitted in Article IV, Section 7 (b) and (c) of this zoning ordinance.
3. Any retail business permitted in Article IV, Section 10.
4. Restaurant (including entertainment and/or liquor).
5. Any wholesale business or storage as described in Article IV, Section 12.
6. Any industrial use as described in Article IV, Section 13.
7. Any facility primarily engaged in the creation or operation of information technology or financial services.
8. Any commercial office or office building not otherwise included in this Section 19 – Planned Development Park.
9. Any facility primarily engaged in scientific, medical, or technological research.
10. Any indoor recreational facility.
11. Any renewable energy facilities covered under the Siting Standards for Renewable Energy Facilities, Article (number to be assigned), Section (number to be assigned).

F. *Prohibited uses in a PDP.* The following uses are prohibited in a PDP:

1. All prohibited uses as described in Article IV, Section 15 shall be prohibited in a PDP.

G. *Prohibited activities in a PDP.* The following activities are prohibited in a PDP:

1. No materials, manufactured items, supplies or equipment shall be stored in any area on a lot except inside a closed building or behind a visual barrier screening such areas from the view of adjoining property and/or a street. No storage or parking of goods, vehicles, and/or equipment shall be permitted on the street. In addition, no portable structures, including but not limited to trailers, storage units or similar objects, shall be permitted within the PDP except during periods of construction.
2. There shall be no excessive noise, dust, smoke, fumes, odors, vibrations, glare, vermin, or liquid or solid waste which is not kept in appropriate containers. There shall be no uses that will result in unsafe or hazardous conditions such as toxic or noxious materials, fire and explosion hazard materials, or radiation hazard materials.

H. *Development Categories* within a PDP may include the following:

1. *Single User.* This category shall include one owner or tenant, located within a primary structure on a single site. The site may also include accessory buildings associated with the primary use.

2. *Building Complex.* This category shall include multiple owners or tenants, located within a primary structure on a single site. The site may also include accessory buildings associated with one or more of the uses or tenants within the primary building.
 3. *Site Complex.* This category shall include multiple owners or tenants, located within multiple primary buildings on a single site. Each building may contain one or more primary users or tenants. The site may also contain accessory buildings associated with any of the owners or tenants within the primary buildings on the site.
- I. *Dimensional Regulations.* The following dimensional regulations shall apply within a PDP and supplant those contained within Article V of these regulations.
1. *Minimum Lot Size:* 40,000 square feet
 2. *Minimum Lot Width:* 120 feet
 3. *Minimum Lot Frontage:* 120 feet
 4. *Minimum Front Yard Setback:* 20 feet
 5. *Minimum Side Yard Setback:* 30 feet
 6. *Minimum Rear Yard Setbacks:* 40 feet
 7. *Maximum Building Coverage:* 70%
 8. *Maximum Building Height:* 65 feet
 9. *Maximum Accessory Building Height:* 35 feet
- J. *Signage Regulations.* The intent of the signage regulations within this section are to control the location, number, size, placement, and other features of signs within a PDP to ensure that all signs are appropriate for their use, benefit the public's safety and convenience, are compatible with their surroundings, reinforce any associated building's design, and add to the unique identity of the park.
1. *Sign Categories:* The four basic categories of signs that are regulated within this section are: a). Complex, Building, or Business Identification; b). Information, Direction or Wayfinding; c). Temporary; and d). Development Park Identity, Gateway and Information.
 - a. *Complex, Building, or Business Identification Signs* are limited to information which identifies the name of the complex or building(s); street address; and the name, tagline, and logo of individual businesses.
 - b. *Information, Directional, or Wayfinding Signs:* On or off-premise signs that provide information for the convenience and safety of the public such as the location of business entrances, exits, parking lots, traffic direction, speed, height or weight limitations or any other instance where signage is necessary for orderly traffic movement and public safety.
 - c. *Temporary Signs* are limited to building or site construction information, or building or site sales or lease information.

- d. *Development Park Identity, Gateway, and Information Signs* shall serve to provide prominent identity and exposure to the park, a suitable gateway into the park, and information such as park layout, maps or directory.
- 2. *Permitted Number and Copy* The number of Complex, Building, or Business Identification Signs for each site shall be limited to the following, based on the type of development as described in G (1-3) of this Section.

- a. *For Single Users:* Each parcel shall be limited to one freestanding (monument type) sign along its primary street frontage as well as one additional freestanding (monument type) sign (at 75% of the maximum height and 50% of the maximum size limitation) for each secondary frontage thereafter, providing that any secondary street frontages contain public vehicular entrances. The freestanding sign information shall be limited to company name, logo, tagline and street address.

The primary building within a parcel shall be permitted one attached sign along the elevation of the primary street, and one additional attached sign (at 50% of the maximum size limitation) along any elevation fronting a secondary street. The attached sign copy shall also be limited to company name, logo, tagline and street address. Accessory buildings will be permitted one attached sign, at 25% of the maximum size, containing building name and number.

- b. *For Building Complex:* Each parcel shall be limited to one freestanding (monument type) sign along its primary street frontage as well as one additional freestanding (monument type) sign (at 75% of the maximum height and 50% of the maximum size limitation) for each secondary frontage thereafter, providing that any secondary street frontages contain public vehicular entrances. The freestanding sign information shall be limited to building name and street address, as well as the name of the individual businesses and their logos with a consistent background and color scheme.

The primary building within a parcel shall be permitted one attached sign along the elevation of the primary street, and one additional attached sign (at 50% of the maximum size limitation) along any elevation fronting a secondary street. The attached sign copy shall be limited to building name and street address, as well as the name of the individual businesses and their logos with a consistent background and color scheme. Accessory buildings will be permitted one attached sign, at 25% of the maximum size, containing building name and number.

- c. *For Site Complex:* Each parcel shall be limited to one freestanding (monument type) sign along its primary street frontage as well as one additional freestanding (monument type) sign (at 75% of the maximum height and 50% of the maximum size limitation) for each secondary frontage thereafter, providing that any secondary street frontages contain public vehicular entrances. The freestanding sign information shall be limited to site complex name and street address, as well as the name of the individual businesses and their logos with a consistent background and color scheme.

The primary buildings within a parcel shall be permitted one attached sign along the elevation of the primary street, and one additional attached sign (at 50% of the maximum size limitation) along any elevation fronting a secondary street. The attached sign copy shall be limited to building name and street address, as well as the name of the individual businesses and their logos with a consistent background and color scheme. Accessory buildings will be permitted one attached sign, at 25% of the maximum size, containing building name and number.

3. *Setback.* The setback for Complex, Building, or Business Identification Signs shall comply with the following:
 - a. Front yard setback: 5 feet
 - b. Side yard setback: 10 feet
 - c. Vision Clearance: Signs must be placed so as not to obstruct sight lines at entrances and exits.
 - d. Street Intersection Triangle: No sign, other than traffic safety signs, may be placed within 35 feet of any street intersection along the frontage lines.
4. *Complex, Building, or Business Identification* sign dimensional requirements. The following limitations shall apply:
 - a. Freestanding (monument type): Maximum Height (measured from the street grade the highest point on the sign) shall not exceed 15 feet. Sign copy shall be placed a minimum of seven (7) feet above grade. Maximum Area (excluding base and area below sign copy) shall not exceed 60 square feet.
 - b. Attached Building Signs: The height of attached building signs shall not exceed the height limit for the PDP or extend more than 10 feet above the building's cornice line (defined as the intersection of the top of the exterior wall and roof plane). The area of attached signs shall be limited to 5% of the building's frontage area (length of building along the primary frontage multiplied by the cornice height). Attached building signs must also meet the minimum standards for legibility and readability from frontage roadways, taking into account building setback from roadway, vehicle speed, viewing angle, and copy. In the interest of public safety, where legibility and readability cannot be met within the 5% building frontage area limitation, the sign copy may be increased to meet minimum standards for legibility.
5. *Development Park Identity, Gateway and Information* signs dimensional requirements. The following limitations shall apply:
 - a. Identity and Gateway: Maximum Height (measured from the ground to the highest point on the sign) shall not exceed 25 feet. Maximum area of sign copy shall not exceed 200 square feet. Any copy intended to be read from adjacent highways must consider setback, traffic speed, and viewing angle to ensure sign copy can be discerned without being considered a distraction. Copy shall be limited to park name and logo.
 - b. Information Signs: Maximum Height (measured from the ground to the highest point on the sign) shall not exceed 12 feet. Other than the information kiosk sign at the entrance containing park map, tenant listings, and addresses), sign copy on information signs shall be a minimum of seven (7) feet from the street grade along the area in which they are placed. Maximum sign area shall not exceed 120 square feet, excluding any area below the minimum copy standard.
6. *Information, Directional or Wayfinding* sign dimensional requirements. The following limitations shall apply:
 - a. All signs related to traffic safety shall meet the minimum Manual of Uniform Traffic Control Devices (MUTCD) standards and be exempt from the limit on freestanding signs per site. Entrance signs, shall not exceed exceed 5 feet in height or 15 square feet, and may also contain the business, building or complex name and street address. These signs do not count against the maximum number of freestanding signs per lot.

7. *Prohibited Signs:* The following types of signs shall be prohibited within a PDP:

- a. Animated moving, rotating or sound emitting signs;
- b. Electronic Message signs;
- c. Reader boards or changeable copy signs;
- d. Flashing signs other than those used for traffic safety;
- e. Internally illuminated signs other than those in which only the letters or logos are internally illuminated individually;
- f. Highly reflective signs that can create disability glare that is determined by the building inspector to be either a public nuisance or public safety hazard;
- g. Inflatable signs and objects;
- h. Window signage;
- i. Portable signs, with the exception of authorized temporary signs;
- j. Moving signs, including but not limited to pennants, flags with commercial messages, streamers, propellers, discs and searchlights;
- k. Signs mounted, attached, or painted on motor vehicles, trailers and used as additional advertising when not conducting a business or service; and
- l. Any sign type that is not specifically addressed in this ordinance will require a special use permit from the Tiverton Zoning Board of Review.

7. *Illumination Methods:*

- a. Externally Illuminated signs may include ground or up-lighting, integral lighting, halo lighting, down lighting. All lighting should be designed to minimize glare, light spill, and provide no more illumination than is required for sign legibility at night.
- b. Internally Illuminated signs where the entire sign board and message are illuminated from a light source within the sign are prohibited. However, if the sign background does not allow light to pass through, individual letters or logos may be internally illuminated.
- c. Neon, LED or other light sources may be permitted where it can be demonstrated that their use furthers the goals within the adopted Standards and Guidelines for the PDP.

K. *Site Regulations*

1. All site designs shall have adequate provisions for employee and customer parking as well as loading areas, and otherwise comply with parking and loading requirements as enumerated in Article X.
2. Loading, service, utility and mechanical equipment shall be located towards the back of the building, or least visible area, and screened from public view.

3. All approved off-street loading facilities shall be paved with asphalt or concrete with adequate drainage and curbing. Where practical, pervious pavement should be utilized. Loading areas shall be located to the rear of any building wherever possible. Loading areas in the rear of side yards shall be screened by substantial means to minimize exposure from the street.
4. No trash facility (dumpsters, recycling areas, etc.) shall be located forward of the primary structure. Such facilities shall be located in the rear of the site (where practical) and be placed in compatibly designed outbuildings or utilize screening to minimize their visual impact.
5. All site design shall have adequate provisions for fire lanes and emergency access, and provide building separation that meets both the applicable building and fire codes.
6. Site design shall meet all of the applicable local and state standards for stormwater management and erosion and sediment control.
7. Where practical, site design and building placements shall make use of existing topography and land features, to minimize the amount of cut and fill required.
8. All sites shall provide safe means of vehicular ingress and egress from a public road with access drives and parking areas design to ensure that vehicles are not required to back out onto public roads.
9. Minimum driveway width for two-way traffic shall be eighteen (18) feet wide; minimum driveway for one-way traffic shall be ten (10) feet.
10. Where practical, sites should provide internal circulation links to adjoining sites.
11. All permanent utility lines shall be located below grade.
12. Sidewalks shall be installed along the frontage of all parcels, within the public right-of-way, to create pedestrian connections to adjoining parcels. Sidewalks shall comply with the design standards in the PDP's Development Standards and Guidelines.

L. Landscaping Requirements

1. All developed land not covered by buildings, parking areas, driveways or other site improvements shall be appropriately landscaped. The owner of any lot shall at all times keep the property, including undeveloped areas, in good order and condition and properly maintained.
2. A Landscape Plan stamped by a Registered Landscape Architect, licensed in the State of Rhode Island is required for all development plans. Such plans shall include the location of all landscaped areas with the proposed trees, shrubs or other plant or ground covering material with information on the size, type and spacing.
3. All planting beds shall have an a minimum two foot uninterrupted boarder of grass or dense ground cover wherever landscaped areas abut paved areas to minimize mulch, topsoil, or sediments from being washed into drainage structures or systems.
4. Landscape plans shall utilize existing site features including existing stone walls, mature trees, site topography, and vegetated buffers to the greatest extent practical.

5. All individually developed parcels shall provide a minimum 10-foot wide landscaped planting strip between abutting properties.
6. All individually developed parcels shall provide a minimum 15-foot wide landscaped planting strip between the front and side (where it is on a corner lot) of the building(s) or any paved area and roadway rights-of-way.
7. Street trees, with a minimum caliper of two (2) inches, shall be required for every twenty-five (25) feet of frontage along the public right-of-way.
8. Parking lot landscaped islands or planting beds shall be a minimum of five (5) feet wide.
9. A landscaping maintenance agreement shall be submitted to provide for maintenance of plantings utilizing acceptable horticultural practices and for replanting of new material where required plantings have not survived.

M. *Building Design Requirements.*

1. Primary building entrances shall be prominently located, easily identifiable, and relate to human scale.
2. Buildings shall be designed and oriented with an emphasis on the primary street on which they front.
3. Buildings located on corner lots shall incorporate design elements that are respectful of their orientation.
4. All mechanical, electrical, or utility equipment, service tie-ins or panels shall be adequately screened from public view utilizing materials and techniques that either blend in with or add architectural interest to the building.
5. All rooftop mechanical or other equipment shall be adequately screened from public view utilizing materials and techniques that either blend in with or add architectural interest to the building unless it can be demonstrated that the equipment itself adds visual interest and furthers the goals in the adopted Development Standards and Guidelines.

N. *Lighting.*

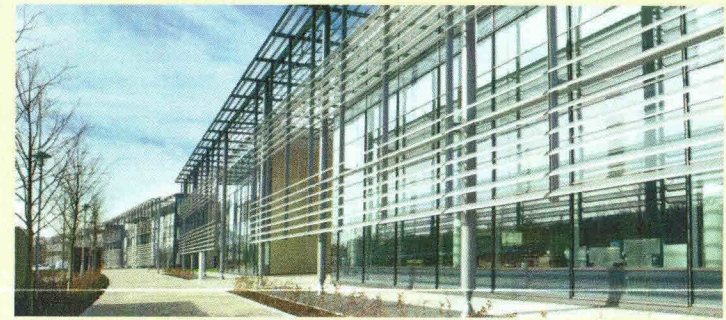
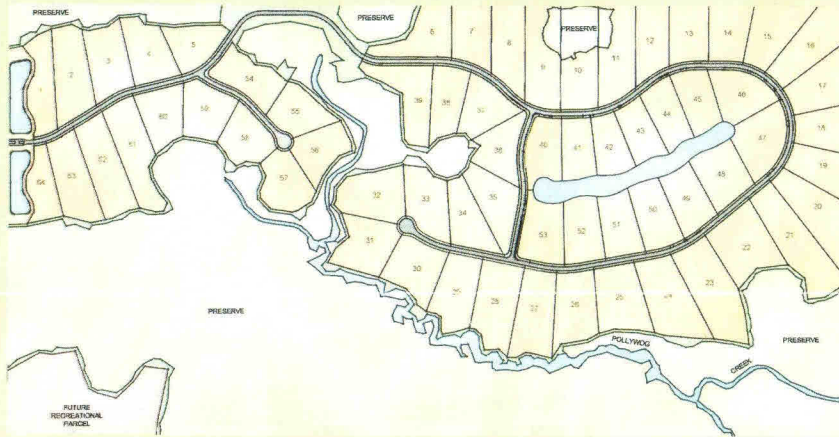
1. Parking lot, vehicular and pedestrian circulation paths, and loading areas shall have adequate lighting for public safety. Lighting should be designed to minimize light spill or trespass light and to add interest and variety to the site.
2. Mounting height for parking lot lighting fixtures shall not exceed 24 feet, measured to the center of the luminaire.
3. Use of landscape, circulation and architectural lighting should be utilized rather than wall mounted security lighting or wall packs.
4. Building lighting and architectural lighting should incorporate a variety of techniques and sources which minimize light spill, highlight architectural features, provide additional safety, and add interest and variety to the building(s) and site.

5. All development proposals shall include a lighting plan that indicates fixture locations, type light source type, and photometric data.

O. *Review Procedure.*

1. Proposed land development projects within a PDP shall be reviewed for their consistency with Tiverton's Zoning Ordinance and the adopted Development Standards and Guidelines for the specific PDP. Proposed land development projects are subject to Land Development Plan Review under Article XX of this zoning ordinance.
2. Expedited Design Review: Projects whose proposed architectural design are deemed by the Building / Zoning Official, Town Planner, Planning Board Administrative Officer and Planning Board Chairman to be in compliance both with this zoning ordinance, as well as meeting the intent and spirit of the Development Standards and Guidelines shall be issued a Certificate of Appropriateness and be granted an administrative approval.
3. Expedited Site Plan Review: Projects whose proposed site design are deemed by the Building / Zoning Official, Town Planner, Planning Board Administrative Officer, Planning Board Chairman and the Town's consulting engineer to be in compliance with this zoning ordinance, all of the applicable state requirements as well as meeting the intent and spirit of the Development Standards and Guidelines shall be issued a Certificate of Appropriateness and be granted an administrative approval.

TIVERTON ENTERPRISE PARK



DEVELOPMENT
STANDARDS &
GUIDELINES

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SECTION 1.0

INTRODUCTION AND STATEMENT OF PURPOSE

The Tiverton Industrial/Technology Park is comprised of 177 acres of land directly abutting Interstate Route 24 and located within a Planned Development Park (PDP) zone. The Park is located approximately 23 miles east of Providence, 30 miles from TFG Airport, 60 miles south of Boston, and 45 miles west of Cape Cod.

The Park was envisioned and is being developed as a “Green Park” combining walking trails, open space, commercial and industrial sites, and number of potential wind energy facilities. The Park will utilize best practices for Low Impact Development (LID), high quality site and facilities design, including LEED or other energy efficiency and environmentally sustainable certification programs and/or construction practices. The layout of the park was engineered to offer a mix of parcels sizes to accommodate a broad range of development interests, taking advantage of topography, site constraints and existing infrastructure, as well as minimizing additional infrastructure costs and allowing for parcel mergers for businesses anticipating future expansion.

The Park will primarily serve the needs of the light industrial, original equipment and manufacturers (OEM) and regional suppliers directly related to the marine, defense and the building trades industries. However, its zoning, as part of a Planned Development Park (PDP) gives greater flexibility and an expanded list of permitted uses, such as restaurants, hotels, or recreational facilities which will compliment industrial, manufacturing, or technology companies that might locate there.

Purpose

The development standards are fashioned to further the following purposes:

- To promote economic development and economic diversity by attracting and retaining a wide range of businesses to Tiverton;



Fig 1.01

Example of an office building exhibiting many of the characteristics that contribute to its industrial aesthetic including exposed steel and glass, horizontal emphasis and exaggerated structural details.

- To clarify the regulatory review process by providing potential tenants with guidance and understanding of the town's visions and expectations as well as a path for expedited review and approval;
- To provide opportunity for new and existing businesses in a location where growth is both possible and encouraged and where public utilities and transportation links are available.;
- To encourage compact development patterns in which denser and more intense forms of development will occur near public services and away from residential neighborhoods;
- To establish highly functional design standards that result in interesting, unique, visually aesthetic developments that celebrate industry and technology, protect the health and safety of the public, respect the existing landscape and environment while enhancing the long-term value and viability of businesses and their property; and
- To assure conformity with the goals and objectives of Tiverton's Comprehensive Community Plan.



Fig 1.02

Example of high quality public improvements in landscaping, sidewalks, and roadways that are designed to be harmonious with the existing natural and individual site landscapes and environments.

SECTION 2.0

SITE PLANNING & DESIGN

Good site planning should strike a balance between maximizing efficiencies and economies for development and minimizing disturbance to natural landscapes by utilizing existing topography and site features to provide interesting, unique, varied, and sustainable design throughout the Park. Site planning should take into consideration the surrounding context, create harmonious relationships with adjoining landscapes, integrate and coordinate access as well as create interconnectivity for vehicular, pedestrian and bicycle circulation to the greatest extent practical.

Building Siting and Orientation

Buildings should be sited and oriented to minimize site disturbance, take advantage of existing topography and natural features, minimize impervious surface and resulting stormwater runoff, and provide a strong relationship between the main entrances and more public portions of the buildings and the street. Parking between the front of the building (and side where it is on a corner lot) and the street should be minimized. Service entrances, loading docks, dumpsters, or other utility infrastructure should be placed so as to minimize any negative visual impact from public areas.

Successful site planning will:

- Incorporate Best Management Practices (BMPs) for Low Impact Development (LID) and meet or exceed all Rhode Island Department of Environmental Management (RIDEM) stormwater management standards;
- Integrate water quality features such as bioswales, retention and detention basins into the overall site design and landscape, taking advantage of topography, existing features, with strong consideration for aesthetics and visual impact;



Fig 2.01

Runoff redirected into integrated water quality and landscape features as part of the overall site planning utilizing a Low Impact Development approach.

- Take advantage of existing topography and natural features in developing parking areas and planning building placement to minimize the amount of re-grading and cut and fill;
- Preserve natural features such as stone walls, significant trees, ponds or streams; concentrating development on the least naturally sensitive or significant portions of the site;
- Redirect and incorporate roof runoff into overall site rain harvesting plan, water quality, and landscape features;
- Create a strong visual and functional relationship between the buildings and the site as well as with adjacent sites, landscapes and nearby streets;
- Locate mechanical equipment, trash/recycling, storage areas, loading docks or bays, and equipment service areas in accessory buildings that have a strong design relationship with the main buildings, or away from portions of the site that are highly visible from public streets or properties with dissimilar uses; and
- Provide for underground utilities in accordance with Article 19 of Tiverton's Zoning Ordinance, Planned Development Park.

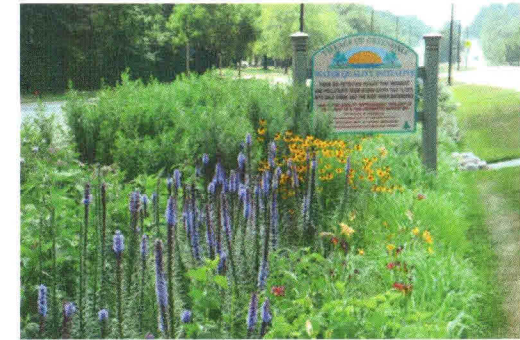


Fig 2.02

Vegetated swale utilized to improve water quality generated from road and nearby sidewalk stormwater runoff, also providing an aesthetically pleasing buffer between the roadway and pedestrian paths.

Fig 2.03

Well integrated landscaping, knitting together parking area, pedestrian and vehicular circulation, and building structure.

SECTION 3.0

CIRCULATION, PARKING AND LOADING

The following Standards and Guidelines for Circulation, Parking and Loading have been developed to ensure that maximum efficiency and safety is maintained among and between all modes of transportation in a manner that contributes to the attractiveness and sustainability of each site as well as the Park.

Circulation Standards

All internal circulation shall comply with the following standards:

- Internal circulation patterns as well as the location and traffic direction of all access drives shall be designed in accordance with accepted principles of traffic engineering and traffic safety;
- Pedestrian access to primary building entrances shall be physically or visually separated from automobile and truck circulation routes as well as loading areas; Sidewalks along street frontage shall have a minimum paved width of six (6) feet;
- Internal site circulation shall be designed so that vehicles are not required to enter onto public streets to gain access to separate areas of an individual site;
- Access drives and internal circulation must be designed to ensure that vehicles are not required to back out onto public streets;
- Two way access drives shall have a minimum width of 18 feet. One way access drives shall have a minimum width of 10 feet;
- All individual sites shall have adequate provisions for emergency vehicles; and
- Circulation, parking, and loading pavement shall be reduced to the minimum amount necessary to safely and attractively accomplish needs. Utilize pervious pavements where practical.



Fig 3.01

This center island provides a safe pedestrian connection to the buildings. The meandering sidewalk allows for more landscaped areas and larger trees than could be accommodated with a curb to curb sidewalk.

Circulation Guidelines

The design of internal circulation should consider the following:

- Curb cuts along roadways should be kept to a minimum to promote vehicular, pedestrian, and bicycle safety while still maintaining efficient circulation and adequate access for emergency vehicles. Curb cuts should be planned to maximize the distance between intersections, roadway curves, and curb cuts on adjoining parcels;
- Shared curb cuts and driveway access between adjoining parcels should be utilized where conditions permit to improve pedestrian and vehicular safety both on the site and along public roadways;
- To promote vehicle, pedestrian and bicycle safety and convenience, and improve fire protection and other emergency services, internal circulation and connections to adjoining lots are encouraged. Where adjoining lots are undeveloped, provisions for future connections should be provided;
- Where practical, curb cuts on opposing sides of the street should be aligned. Where medians exist, curb cuts should align with breaks in the median;
- Pedestrian, bicycle and vehicle circulation should be physically or visually separated, utilizing planting isles, elevation changes, pavement differentiation, pavement striping or other means to achieve this;
- Where practical, major interior circulation routes and access driveways should be physically separated from parking and loading areas to reduce vehicular and pedestrian conflicts; and
- Frontage sidewalks should be located to achieve maximum separation from the roadway, while still within the public right-of-way.
- Long access driveways and internal circulation routes should utilize traffic calming measures or other means to reduce design speed and improve safety.



Fig 3.02

Well marked and differentiated pedestrian pathways through a parking lot and travel ways utilizing pavement markings and different pavement types.

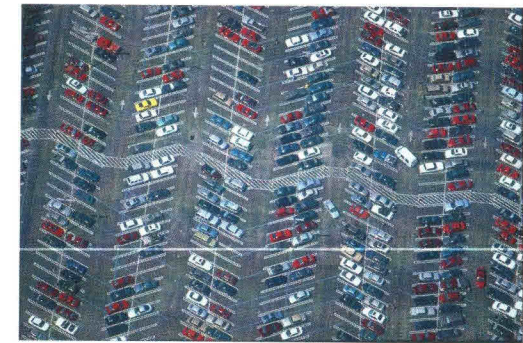


Fig 3.03

Pedestrian paths through this parking lot are well-marked, but are not likely discernible at ground level and would not improve pedestrian

Parking Standards

All internal site parking shall comply with the following standards:

- Visitor, customer and employee parking shall comply with Article X of Tiverton's Zoning Ordinance.
- Parking areas shall be design to ensure visibility for pedestrian, bicyclists and motorists entering individual spaces, circulating within parking areas and entering or leaving parking, loading or building areas.
- Parking shall be designed to ensure that vehicles are not required to back out onto public roadways.
- Dead end isles are prohibited unless adequate provisions for vehicle turnarounds are provided.
- The majority of the parking shall be placed behind the buildings or along the side, except where that side fronts on a public street. A maximum of two rows of parking in front of the building or along the side where it fronts a public street will be permitted for customers or visitors.
- Parking areas shall be treated as well-defined spaces that integrate landscaping, lighting, and pedestrian and vehicular circulation.
- Parking lots shall not be the dominant visual element of any site.
- Uninterrupted rows of parking shall not exceed twenty-five (25) bays without landscaping breaks or treatments.
- In complying with Article X of Tiverton's Zoning Ordinance, up to twenty percent (20%) of the parking may utilize eight (8) foot by sixteen (16) foot parking bays for compact cars.
- Motorcycle parking and bicycle racks shall be incorporated into the overall parking plan.

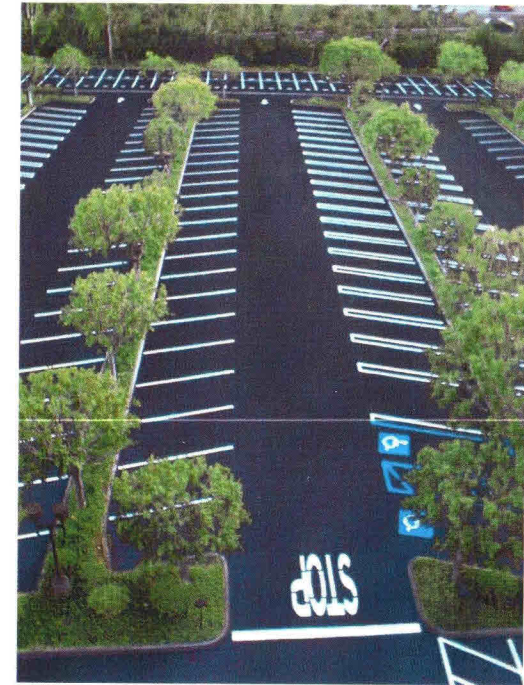


Fig 3.04

Parking lot exhibiting a mix of landscaped islands, trees, shrubs, groundcover and border plantings.

Parking Guidelines

The Design for internal site parking should consider the following:

- Large parking areas should be broken up into smaller connected areas with large landscaped islands and peninsulas, natural buffers, and where appropriate utilize existing topography and natural stone walls to create terraced parking areas;
- Parking area landscaping should be integrated into the overall site design, internal circulation, landscape plan, lighting plan and Low Impact Development (LID) stormwater management plan;
- Shading devices such as large canopied trees, solar panels, and rain collection systems should be utilized where appropriate and should contribute to the overall appearance of the site;
- Pedestrian connections, either physical or visual should link parking areas to the buildings to decrease vehicular / pedestrian conflicts;
- Parking along the front or side where it abuts a public street should be softened with the use of low retaining walls, low shrubs, ornamental trees or utilize existing topography to minimize the visual impact. Fencing, arborvitae or other screening devices which interrupt the view should not be utilized;
- Parking lots should be designed to ensure that long travel isles do not contribute to vehicle speed;
- The orientation of parking areas should not require pedestrians to make multiple crossings over vehicle travel lanes to go from the edge of parking lots to the buildings; and
- Parking lots should utilize existing topography to minimize cut and fill and to break up parking fields into smaller components. Where practical, local stone from the site should be utilized for retaining walls at parking area grade changes.

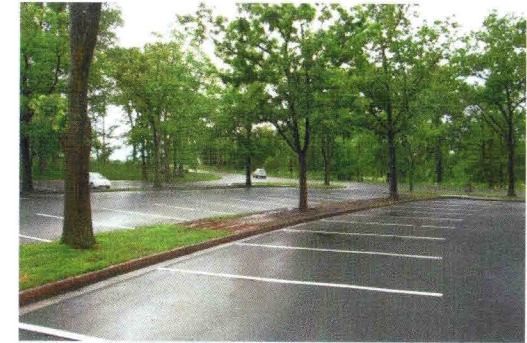


Fig 3.05

Large mature trees incorporated into parking lot design to break up parking areas and provide shade.



Fig 3.06

Artist rendering depicting solar trees used to provide parking lot shade and alternative energy.

Loading Area Standards

The loading areas shall comply with the following standards:

- Loading areas shall not be located along the front of the building, or along a side where it abuts a public street;
- Loading areas shall be designed to ensure that trucks are not required to back out onto a public street, or back in from a public street;
- Loading areas shall be design to ensure that trucks in the loading area do not block fire lanes or other major vehicular ingress or egress routes;
- Loading areas shall be located away from primary building entrances or other areas of pedestrian activity;
- Loading areas shall be either located away from areas of public view, screened utilizing screening that adds interest and is compatible with the overall building design, or integrated into the overall building design;
- Loading areas shall have lighting that is adequate for safety and which adds to the overall positive visual impact of the building and does not create a nuisance for abutting property owners; and
- Loading areas shall be adequate for the proposed operations of the building.



Fig 3.07

Unattractive loading area combined with unscreened dumpsters, electrical service panels, and storage area with fencing that does not match the character of the building.



Fig 3.07

Loading area that matches the character of the building, designed to keep trucks out of the main circulation path of other vehicles.

Loading Standards

The design for the loading areas shall consider the following:

- Roofing designs and structures that adds to the overall industrial aesthetic of the building for protection;
- Recessing the loading dock and doors into the building, creating protection and breaking up the mass of the building, while providing visual interests;
- Minimal exterior access doors with the majority of loading dock space on the interior of the building;
- Where practical, loading areas should provide landscaping to break up large areas of pavement;
- Where delivery or loading requirements are minimal, the main public entry may be utilized if it is integrated into the overall design; and
- Loading areas should minimize paved areas to the greatest extent possible and incorporate pervious pavement where practical.



Fig 3.08

Attractive loading area that is integrated with the overall design of the building.

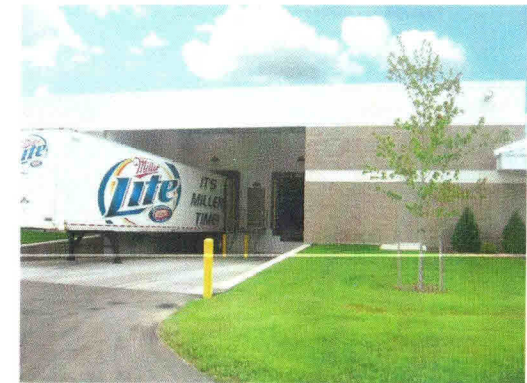


Fig 3.09

Loading area recessed into the building for protections and landscaped to minimize

SECTION 4.0

LANDSCAPE DESIGN

The following Standards and Guidelines for Landscape Design have been developed to encourage site development that adds to the overall visual appeal of the entire Park, blends into the existing environment at the edges, reinforces the public realm, and provides a neutral backdrop and strong contrast to the industrial aesthetic of the building designs.

Landscape Design Standards

The Landscape Design shall comply with the following standards:

- The site and landscape design shall be integrated into the overall stormwater management plan;
- Walls and fences shall be of a scale and material appropriate to the site and surrounding areas. Fences in close proximity to the buildings should be reflective of the overall design intent;
- Landscaping Plans shall comply with Article IV, Section 19 of Tiverton Zoning Ordinance and differentiate between planned and maintained areas of landscaping and areas to be left in an undisturbed state;
- Street trees shall be planted along all property frontages in conformance with Article IV, Section 19 of Tiverton Zoning Ordinance and the Recorded Plan for the Tiverton Enterprise Park; and
- All areas of a site not containing buildings, driveways, parking, sidewalks, trails, or being left undisturbed must be landscaped with live plant material—utilizing a combination of trees, shrubs, groundcover, and perennial and annual type flowers. Mulch and decorative stone do not constitute live plant material and should not be used as a major landscape element.



Fig 4.01

Neutral landscape providing strong contrast with building materials and overall design intent.

Landscape Design Guidelines

The Landscape Design shall consider the following guidelines:

- Utilize materials found on the site such large boulders for landscaping features or retaining walls. Process excess rock on site for use in drainage areas;
- Incorporate existing stone walls into the overall site and landscape plan where practical. Restore, relocate or rebuild stone walls found on site as necessary—connect with stone walls on adjacent sites to the greatest extent possible;
- Restore and maintain historic cart paths or walking trails through the site for walking trails, coordinate with adjacent properties to link trails throughout the site for the recreational enjoyment of all park tenants;
- Take advantage and incorporate natural site features such as rock outcrops, mature trees, or clusters of smaller trees into the overall landscape plan;
- Plant native, drought tolerant plants to maximize water conservation and minimize the use of pesticides;
- Ensure that overall landscape design is based on a year round approach in terms of interest—utilizing shrubs, plants, and trees that flower and change color throughout the year. Consider how shrubs and trees appear without leaves, and consider ones that have interesting and varied sculptural qualities to add interest to the winter landscape;
- Redirect and incorporate roof runoff into overall site rain harvesting plan, and utilized rain gardens, and bioswales to mitigate stormwater runoff and improve water quality; and
- Create a strong visual relationship between adjacent landscapes and develop logical transitions into undisturbed buffers or landscapes.



Fig 4.02

Vegetated swale utilizing crushed rocks, found on and processed on site should be considered.

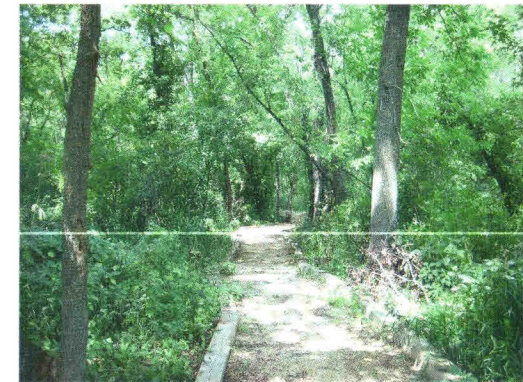


Fig 4.03

Existing walking trails should be retained or new ones created throughout the park.

SECTION 5.0**ARCHITECTURAL DESIGN**

The design guidelines and standards for buildings within the Park are intended to promote an industrial image or aesthetic that celebrates manufacturing, industrial design, and technology. Rather than require industrial buildings to be by clad in brick, stone, or stucco—pretending to be something they are not—this aesthetic seeks to be more reflective of the industrial, technology, or manufacturing uses taking place within the buildings and the Park.

This industrial look or aesthetic should emphasize the engineering, structure and design elements such as stair towers, bays, entrances, rooflines, and corners utilizing extended girders, prominent corners posts, or exposed framing and be rendered using industrial materials such as metal glass, stainless steel, and other high quality materials and finishes associated with contemporary industrial buildings.

**Fig 5.01**

Industrial aesthetic showing strong horizontal banding, extended rooflines, and use of industrial materials.

Fig 5.02

Buildings should exhibit an emphasized front or public section with strong design elements, materials and finishes that contribute to an industrial aesthetic. A simpler more utilitarian design towards the back is appropriate.

Architectural Design Standards

The building designs shall comply with the following standards:

- Buildings shall be designed and oriented with an emphasis on the primary street on which they front, utilizing design elements, materials, and finishes that contribute to the building's individual and the parks overall industrial / technology aesthetic.
- Buildings located on corner lots shall be respectful of their orientation and utilize design elements, materials, and finishes associated with the industrial / technology aesthetic;
- The primary public entrance shall be prominently located, easily identifiable, relate to human scale, and contribute to the overall design intent;
- Primary public entrance shall be at grade or seamlessly integrate handicap accessibility into the design of the building; and
- Rooftop mechanical or other equipment that does not contribute to the overall design intent must be screened from public view utilizing screening techniques that either blend with the building or compliment its design.

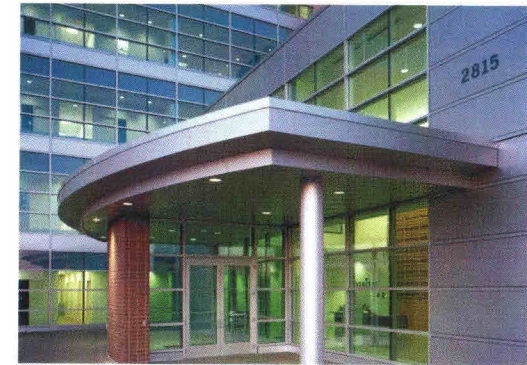


Fig 5.03

Well design public entrance which provides shelter, relates to the human scale, and is consistent with the overall design intent.

Fig 5.04

Many elements that are often utilized in LEED certified buildings, such as window screening as shown here, are also design elements that contribute to an industrial aesthetic.

Architectural Design Guidelines

The building designs should consider the the following architectural guidelines:

- All sides of a building should be treated with variations in massing and articulation. Vary building height, massing, setback or simplify design elements to define different functions such as office or manufacturing areas;
- Arrange buildings on a site or individual components of a building to create opportunities for plazas, courtyards or other useful outdoor spaces;
- Ensure roofs that are visible are designed to compliment the composition and form of the building;
- Maintain similar style, materials, and proportions, for all buildings on a site—ensuring that each building adds to the overall composition of the site and design intent of the primary building(s);
- Rooftop mechanical or other equipment that does not contribute to the overall design intent must be screened from public view utilizing screening techniques that either blend with the building or compliment its design;
- Utilize projecting features such as architectural canopies, sun shading devices, and exaggerated roof overhangs; and
- Avoid natural materials such as unfinished wood or stucco. Focus more on manufactured or machined materials and finishes such as stainless steel, painted steel, anodized aluminum and other alloys, and glass that project an image related to manufacturing, technology or industry.

Buildings showing a strong industrial aesthetic will often have many of the following characteristics:

- Designs that convey a look of strength, permanence, and refinement of machine or industrial manufactured materials;



Fig 5.05

An industrial or technology aesthetic applied to restaurant or other uses which serve the park would be appropriate and add strength to the image of the Park overall.



Fig 5.06

Applying an industrial aesthetic to utilitarian buildings will help reinforce the image of the Park

- Exposed, extended, or exaggerated structural elements;
- Geometric patterns, often in a grid with a horizontal or vertical emphasis;
- Easily identifiable and integrated main entrances;
- Integrated shading devices for windows—adding interest, texture, and variety during both day and night;
- A harmonious mix of materials and colors, with strong colors used sparingly, often just for major structural elements;
- Interesting and bold shapes;
- Horizontal or vertical banding through the use of material articulation, structural elements, and windows.
- Emphasized structural bays, often protruding or recessed.
- Use of suspended or cantilevered elements
- Dynamic and well-integrated lighting, often emphasizing structure.



Fig 5.07

Exaggerated rooflines, exposed structure, refined materials and variations in massing and setback contribute to the industrial aesthetic of this building.

Fig 5.08

A building that would be appropriate in an industrial / technology park—utilizing horizontal banding, refined materials, and variations in the wall plane.

SECTION 6.0

SIGNAGE

Signs play an important role in the success of any business by providing necessary identification and advertising. Signage within the Park, whether related to traffic flow and safety or business identification, should serve to provide sufficient information in a legible and logical manner in order to safely and efficiently orient and direct visitors to their destination. This should be done through a clear, organized system of signage which is consistent with the overall design intent of the Park and which compliments the design and architecture of individual site developments.

Sign Standards

All signage within the Park shall comply with the following standards:

- Signage shall be kept to the minimum amount and size necessary to adequately convey business identification, directional, or traffic safety information;
- Freestanding signs, shall comply with the design and dimensional requirements of Article IV, Section 19 of Tiverton Zoning Ordinance;
- Signs within the Park shall be placed and oriented to ensure that they do not visually obstruct vehicular, bicycle, or pedestrian traffic;
- Sign copy shall be designed and sized to ensure sign legibility taking into account lettering and background contrast, sign reflectivity, lighting, traffic speed, road type, sign setback and viewing angle;
- Sign lighting shall be designed and oriented to minimize light spill, eliminate disability glare, and reduce discomfort glare to the greatest extent practical; and
- All conductors, transformers, or conduits related to lighting within the Park shall be concealed or hidden from view.



Fig 6.01

Signage materials, colors, and design should be consistent with the overall industrial aesthetic of the Park.

Sign Guidelines

The design of signs should consider the following:

- Freestanding and attached signs should be consistent with the architecture, materials, colors, and scale of the associated development, utilizing the refined simplicity and machined materials associated with the industrial aesthetic;
- Bold colors and complex font designs should be used sparingly;
- The graphic design and layout of sign copy should be related to an industrial / technology aesthetic;
- Utilizing the building materials as a background for individual letters helps integrate the sign with the building and reduces the apparent size of the sign.
- Individual raised letters provide an area for backlighting—creating shadows that contribute to a three-dimensional quality during the day and night.

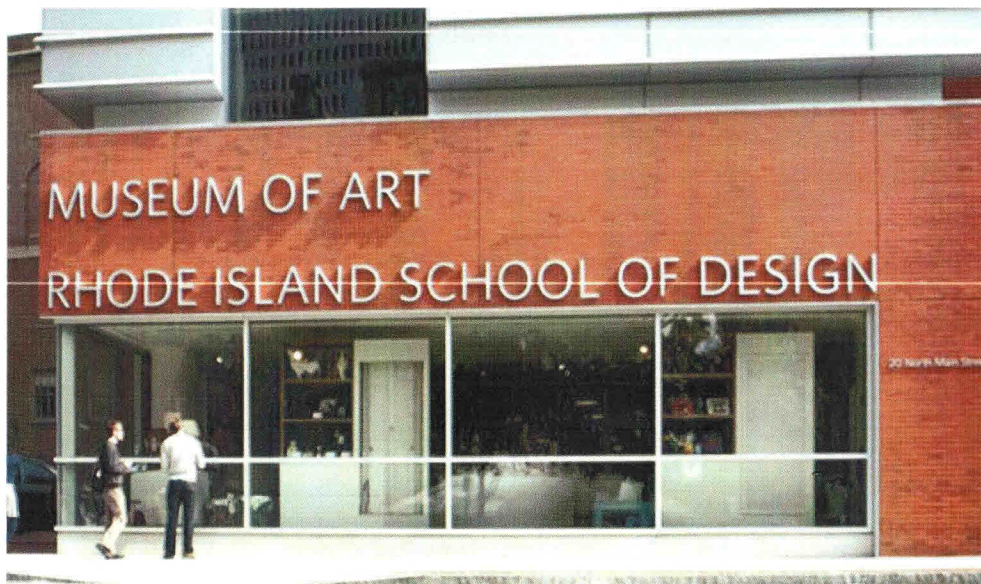


Fig 6.02

Monument sign with minimal information, utilizing refined materials and raised channel letters, providing a simple shadow line and strong contrast.

Fig 6.03

Simple sans serif font with a strong industrial design quality — repeating the materials in the upper portion of the building against the brick to unify the composition of building and

SECTION 7.0

LIGHTING

Exterior lighting plays both an aesthetic and utilitarian or functional role. The functional role is one of building security and safety for pedestrians, bicyclists, and motorists. A good lighting plan will integrate security and safety into the aesthetic role, obviating the need for unsightly security wall pacs or other lighting fixtures or solutions that have a negative visual impact on the nighttime appearance of an individual building, an entire site, or the Park itself.

Lighting should be seen not just as a necessity, but as an opportunity. Proper lighting provides an enhanced and much different view of a building, its landscape or site than is typical during the day. Artificial lighting affords the opportunity to illuminate a building or particular element of a building from a different direction than during the day, completely changing the look of a building. The colors and materials of a building can be transformed or enhanced through proper lighting, and the interior of a building can be revealed at nighttime through the use of proper lighting.

Lighting Standards

All lighting within the Park shall comply with the following standards:

- Utilize efficient fixtures, designed for the specific task to minimize light spill onto adjacent properties or into the nighttime sky;
- Integrate security and safety into the aesthetic considerations of the lighting plan;
- Conceal fixture that do not contribute to the overall industrial aesthetic;
- Utilize full cut-off fixtures in parking lots and other areas away from the buildings; and
- Ensure that all lighting contributes to the positive visual impact of the site.



Fig 7.01

Monument sign with minimal information, utilizing refined materials and raised channel letters, providing a simple shadow line and strong contrast.

Lighting Guidelines

All exterior lighting shall consider the following guidelines:

- Light fixtures should either be inconspicuous or designed to be compatible to the architecture or landscape, depending on their location.
- Buildings, parking areas, walkways, and landscapes should not be flooded with light in attempt to recreate a daytime appearance. Lighting should be used to enhance building texture, highlight structural qualities, reveal interesting interiors, and add ambiance to the landscape and site.
- Lighting along pathways, driveways, and parking areas should spill onto adjoining landscapes to improve security.
- Wall pack security lighting should be avoided since they typically have a negative visual impact, and often contribute to security issues by creating dark areas along the base of a building.
- Sign lighting should be designed to improve the nighttime discernment of signs and ensure readability while minimizing light spill.
- Light sources should be selected on the bases of efficacy, correlated color temperature (CCT) and a high color rendering index (CRI).
- Avoid using high pressure sodium as a light source in landscapes because of its low CRI and tendency to greens in nature as more brown.
- Whiter lights, such as metal halide or LED with a high CRI have a much higher perception of brightness and should be considered in parking areas and along pedestrian pathways.
- Parking lot lighting standards and luminaires should be designed to compliment the industrial aesthetic of nearby buildings on the site. Avoid traditional post-top parking lot lights and ensure that all fixtures are full cut-off to minimize light spill.



Fig 7.02

Interior lighting enhancing the nighttime appearance of building.



Fig 7.03

Exterior lighting can enhance even the most utilitarian structures.

SECTION 8.0: PARKING LOT DO'S AND DON'TS

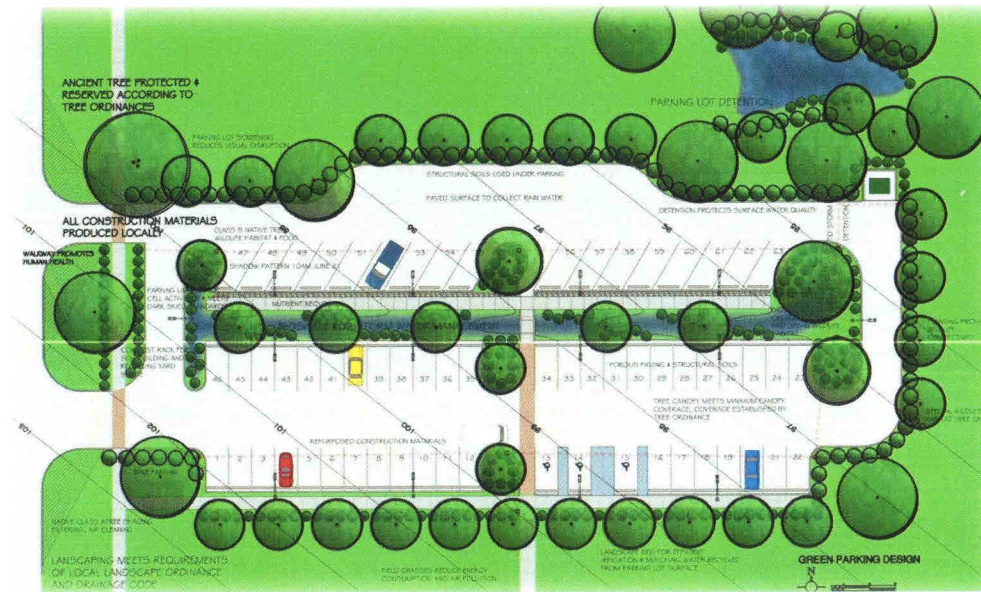


Fig 8.01

DO

Incorporate stormwater management, pedestrian safety, landscaping, and shading into parking lot design.



Fig 8.02

DON'T

Make parking lots the primary focal point or design parking lots without consideration stormwater management, pedestrian safety, landscaping, and shading into parking lot design.

SECTION 9.0: LANDSCAPING DO's AND DON'Ts

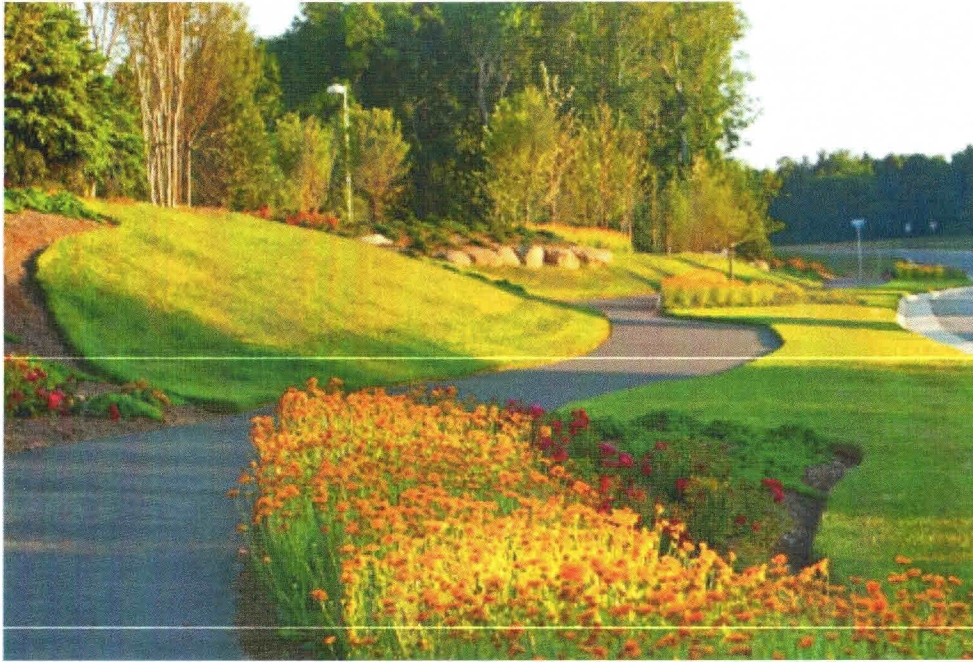


Fig 9.01

DO

Utilize a variety of plants including trees, shrubs, groundcovers, and flowers.

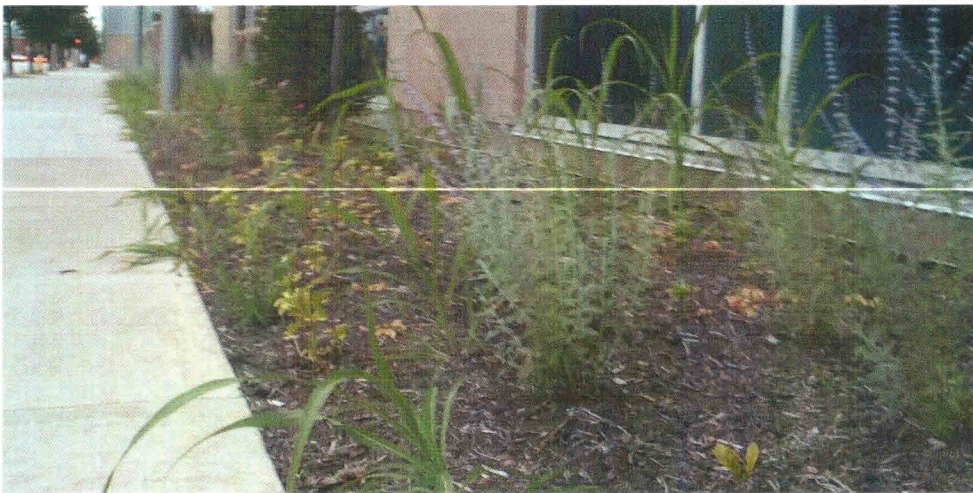


Fig 9.02

DON'T

Create landscaping areas that are primarily dependent on mulch.



Fig 9.01

DO

Take advantage of existing topography and utilize stone from site for retaining walls.

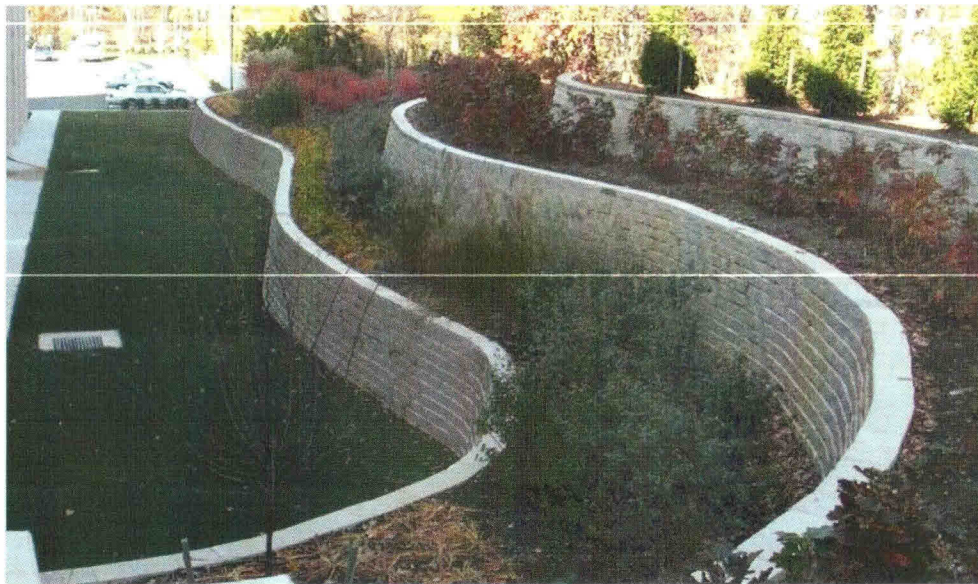


Fig 9.02

DON'T

Create terracing that is too forced or use manufactured modular blocks for retaining walls.

SECTION 10.0: ARCHITECTURAL DESIGN DO's AND DON'Ts

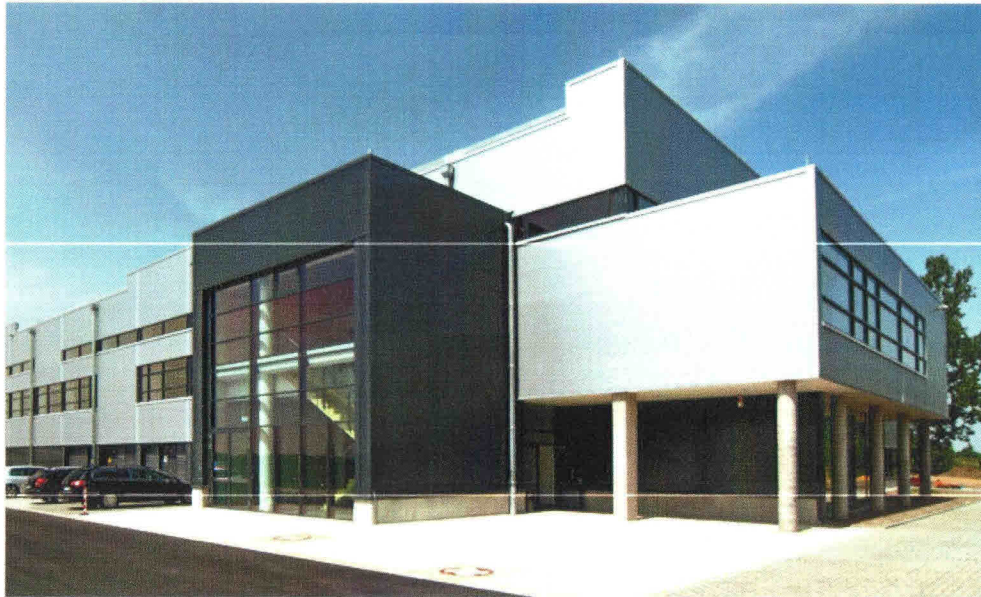


Fig 10.01

DO

Utilize a variety of forms and materials associated with manufacturing and technology.



Fig 10.02

DON'T

Utilize rely on materials not associated with manufacturing and technology.



Fig 10.03

DO

Create them and texture for street views through the use of varied massing, materials and treatments.



Fig 10.04

DON'T

Design blank walls to front streets or other areas with high profile public views.

Fig 10.05

DO

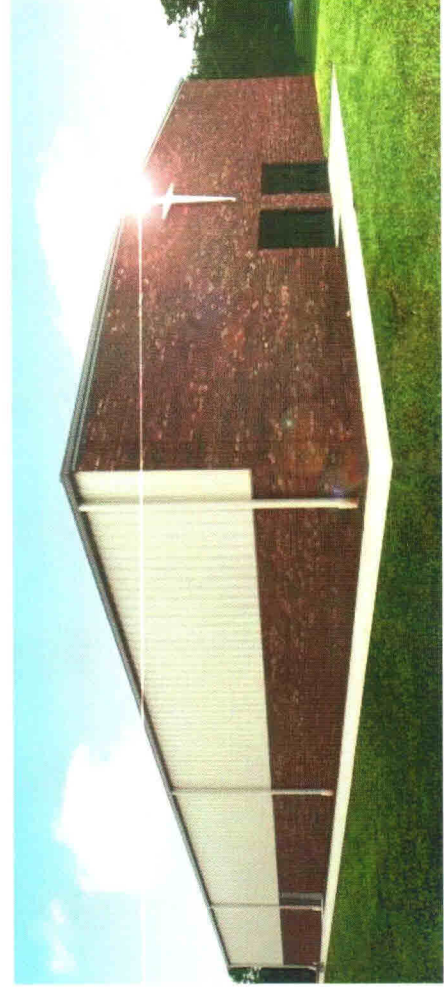
Incorporate materials and designs that have a relationship with the function of the building.



Fig 10.06

DON'T

Utilize materials and designs that have no functional relationship with the building's use.



SECTION 11.0: SIGNAGE DO's AND DON'Ts



Fig 11.01

DO

Create a strong identity for the park that is reflective of the uses and vision.



Fig 11.02 & 11.03

DON'T

Put all tenants on the entrance sign. ON building or site complex signs, utilize a common font and color scheme for tenant names.

#3

Tiverton Planning Board
346 Judson Street • Tiverton Rhode Island 02878
(401) 625-6718



Mailing Address:
343 Highland Road • Tiverton Rhode Island 02878

RECEIVED
TOWN OF TIVERTON
2012 JAN 12 P 2:42

MEMORANDUM

DATE: January 11, 2012
TO: Jay Lambert, Town Council President
FROM: Stephen J. Hughes, Planning Board Chairman
CC: file
RE: Tiverton's Agenda for Action – Status of Planning Board Items

Attached you will find a status update regarding the Planning Board's priorities and areas of focus. These priorities and areas of focus are identified in the policies contained within the Tiverton Comprehensive Community Plan, with the responsible parties and timeline for completion detailed in the Plan's final chapter titled "Tiverton's Agenda for Action". The attached summary lists the policies pertaining to the Planning Board, the time frame for completion and the status of the item.

I will look forward to discussing this document and its contents at the January 30 workshop. If you have any questions or concerns, please do not hesitate to contact me.

**TIVERTON COMPREHENSIVE COMMUNITY PLAN
TIVERTON'S AGENDA FOR ACTION
ADOPTED, JUNE 2009**

REPORT ON STATUS: JANUARY, 2012

TIVERTON PLANNING BOARD

Prepared by Kate Michaud, Administrative Officer

TIME FRAME

SHORT TERM = 1-5 YEARS

MID TERM = 5-10 YEARS

LONG TERM=OVER 10 YEARS

NATURAL AND CULTURAL RESOURCES

Policy: Continue an active program of land acquisition to protect open space and rural character.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: The Planning Board continues to encourage the preservation of open space and rural character through the use of the Rural Residential Development standards. The Tiverton Town Planner is also in contact with the Tiverton Open Space Commission, the Tiverton Land Trust and the Nature Conservancy as needed to facilitate their acquisition programs.

Policy: Work with developers to promote rural residential developments as the preferred type of development pattern.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Tiverton Planning Board and the Town Planner encourage the use of the Rural Residential Development (RRD) standards when working with developers. The Land Development and Subdivision Regulations have been amended to require the presentation of a RRD plan layout for all subdivisions occurring within the primary or secondary areas of the Stafford Pond and Nonquit Pond Watershed Protection Overlay District.

Policy: Use the services of professional design consultants in the design of rural and conservation developments.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board encourages the use of design professionals by developers, and requires the use of RI Registered Land Surveyors, Engineers and/or Landscape Architects as appropriate. A third-party engineering consultant is utilized by the Board (at the applicant's expense) for the review of all aspects of the engineered plan, including road, drainage and landscape design.

Policy: Continue to require protection of existing significant trees, and specify replacement trees for new developments and subdivisions.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board continues to require developers to depict significant trees on subdivision plans through the use of a Site Analysis Map and to encourage the protection of these trees during and post-construction. Tree Commission member Peter Moniz also sits as a Planning Board member, and offers his expertise with regards to tree preservation and replacement.

Policy: Establish a design review commission to assist the Planning Board in approving architectural designs for new commercial and mixed use development.

Policy: Adopt specific design standards for new commercial and mixed use developments in town.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: After much thought and consideration, the Board has decided that a single set of design guidelines applicable throughout all commercial areas of Town would not be feasible or desirable. General design guidelines exist currently for all commercial and industrial development, with more specific standards in place to protect the unique historic character of the Village Commercial (VC) Zoning District. All external alterations of structures or new construction within the VC District, and all significant alterations to structures or new construction within the General Commercial (GC) Zoning District are reviewed by the Planning Board in accordance with Article XX (Development Plan Review) of the Tiverton Zoning Ordinance. An ad-hoc Technical Review Committee is available to developers in order to provide an informal setting to discuss architectural design for new commercial and mixed use development in any Zoning District. In addition, a subcommittee of the Planning Board has been formed to begin the task of creating zone specific non-residential design standards. The subcommittee's first task will be creating a standalone document outlining the design and development standards within a new floating Zoning District, with an eye to the future development of the Tiverton Industrial / Enterprise Park. The Town Planner is a member of the subcommittee, and has coordinated with the Tiverton Economic Development Commission, the Rhode Island Economic Development Corporation and the Newport County Chamber of Commerce. These Standards and Guidelines, as well as the floating Zoning District, are currently under review by the Planning Board. At the conclusion of the Planning Board's review, the documents will be submitted to the Town Council for review and adoption.

Policy: Continue to identify natural and scenic vistas and apply standards for their protection as part of the development process

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: Natural and scenic vistas are identified through the Site Analysis Mapping process, as required by the Land Development and Subdivision Regulations. While vistas are not regulated by the Town, their preservation is encouraged by the Board.

Policy: Consider the establishment of a scenic roadways overlay district.

Planning Board Responsibility: Joint

Time Frame: Mid Term

Status: Possible participation in the scenic roadways program has been explored in the past. The issue has not been recently discussed by the Board.

Policy: Ensure compliance with the Rhode Island Freshwater Wetlands Act.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: For all Planning Board applications, compliance with the RI Freshwater Wetlands Act is required prior to approval.

Policy: Update the Harbor Management Plan as necessary to ensure the protection of environmentally sensitive areas.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: Primary responsibility for implementation of this policy lies with the Harbor Management Commission, which includes a delegate from the Planning Board. The Planning Board and Town Planner would gladly assist the Harbor Management Commission in the update of this Plan.

Policy: Define coastal scenic views and consider standards for their protection.

Planning Board Responsibility: Joint

Time Frame: Mid Term

Status: Implementation of this policy has not been completed.

Policy: Enact and enforce the proposed amendments to the Soil Erosion and Sediment Control Ordinance. Continue enforcement of the existing ordinance.

Planning Board Responsibility: Primary

Time Frame: Short Term / Ongoing

Status: Amendments to the ordinance have been adopted by the Tiverton Town Council. The Tiverton Planning Board participates in the enforcement process when applicable and offers technical support to the Building / Zoning Official and/or Director of Public Works as needed.

Policy: Continue to include evaluation and possible protection of archeological resources in the site plan and subdivision review process.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board continues to monitor archeological resources through the site plan and subdivision / land development review process. Resources provided by the Historical Cemeteries Commission and other applicable outside sources are consulted prior to approval by the Planning Board.

LAND USE PLAN

Policy: Ensure that the land use goal and specific visions for desirable future development are achievable with the zoning and subdivision regulations.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: There is an ongoing effort to bring the Zoning Ordinance into compliance with the Comprehensive Community plan, in order to ensure that future development occurs in a desirable manner. The Planning Board is reviewing the Zoning Ordinance on a district-by-district basis, in order to minimize the cost of the process.

Policy: Evaluate the development potential of the vacant industrial zoning, and consider rezoning.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Planning Board has appointed an ad-hoc subcommittee to draft design standards for a new floating industrial-focused floating Zoning District with an eye to the Tiverton Industrial / Enterprise Park. These floating zone regulations will likely be contained within a stand-alone section of the Zoning Ordinance, and will refer to adopted Design Standards and Guidelines. The intent is to create a streamlined permitting process for projects meeting the zoning requirements and the standards and guidelines. The Planning Board will also review the allowable uses within the Industrial Zoning District and/or the Industrial / Enterprise Park, and will work with the Tiverton Economic Development Commission and others to draft a proposed amendment that best suits the needs of the Town and industrial / commercial users that may wish to locate within the Town. The Town Council will make the final determination regarding any amendments to the Tiverton Zoning Ordinance and/or Zoning Map. This process could be aided through the acquisition of additional GIS software for the Planning Board's use, in order to identify vacant sites by zone.

Policy: Evaluate the uses and dimensional requirements of the Highway Commercial District, and modify if necessary.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Highway Commercial uses and district boundaries were reviewed and amended in 2008. Further revisions may be proposed as the Planning Board continues their zone-by-zone review of the Town.

Policy: Evaluate the Village Commercial District to ensure protection of Tiverton Four Corners.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Planning Board is currently reviewing a proposal, drafted by the Town Planner, which would amend the Village Commercial Zoning District boundaries and allowable uses. There has been much discussion regarding the best way to preserve and support the existing commercial entities, the historic buildings, the area's unique charm and the quality of the Nonquit Pond watershed and individual private water supplies. This has proven to be a very challenging process, and the topic has been tabled at this time to allow the Board to continue work on other areas of Town.

Policy: Consider the application of the Village Commercial District to other areas of town.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Planning Board has considered the expansion of the Village Commercial District to other abutting parcels. Application to other areas of town has not been considered due to the unique character of the Four Corners area.

Policy: Develop a comprehensive waterfront plan that evaluates long term options for development, improved aesthetics and public access.

Planning Board Responsibility: Primary

Time Frame: Mid Term

Status: The Planning Board, through its subcommittee, has just recently started the process of developing a waterfront plan by creating an inventory of existing parcel sizes and uses. The Waterfront Zoning District will be evaluated as part of the Planning Board's zone-by-zone review of the town. The Zone boundaries, dimensional requirements and uses will be evaluated.

Policy: Complete improvements to the Stone Bridge area.

Planning Board Responsibility: Joint

Time Frame: Mid Term

Status: The Planning Board does not have the capability, or the budget, to complete the improvements to the Stone Bridge area. The Town Planner has worked with various local, State and Federal offices to obtain funding to stabilize the Stone Bridge abutment, which acts as a storm surge protector for the Stone Bridge area. The Town Planner has been appointed as the Chairman of the Stone Bridge Committee, and continues to work on the streetscape and abutment improvement plans for the area. The Department of Public Works and the Town Administrator are also points of contact for repairs to the actual bridge abutment.

Policy: Support industrial and commercial uses that result in net tax revenue and are compatible with the small town and rural character of the town.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: To the extent possible under the existing regulations, the Planning Board does weigh possible costs and benefits associated with proposed industrial and/or commercial development. However, it is always difficult to predict tax revenues based on a subdivision or land development plan. Unfinished projects may result in a lower non-residential tax revenue generation than predicted for several recent mixed-use projects.

Policy: Inventory and evaluate town-owned property to determine its best use based on the Comprehensive Community Plan, and review periodically.

Planning Board Responsibility: Joint

Time Frame: Short Term / Ongoing

Status: The Town Council has established a Municipal Buildings Feasibility Advisory Committee, which will accomplish this task. A process for periodic review should be established.

Policy: Establish a planning department with a full-time planner and staff.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: A Planning Department has been established, and a full-time planner, Christopher Spencer, has been employed.

Policy: Develop regulations with regard to the siting of utility and accessory structures.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Planning Board is currently reviewing a draft ordinance pertaining to Wind Energy Conversion Facilities, which might be considered to be utility and/or accessory structures. Siting of structures such as cellular towers is regulated by the Zoning Board. Regulations already exist with regards to residential accessory structures such as sheds and garages. The Zoning Ordinance was amended to allow for detached garages in the side yard of a dwelling.

Policy: Prepare needs assessment for adoption of impact fee ordinance.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: A needs assessment was prepared by consultant Samuel Shamoon and adopted by the Town Council. An impact fee ordinance was also adopted.

HOUSING

Policy: Require developers to consider plans to conserve and complement historic structures.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board requires applicants to identify all existing structures located on a parcel proposed for development or subdivision. Stone walls and other significant features must also be identified. Preservation, relocation and/or reuse of these significant features is encouraged by the Board.

Policy: Explore a broader range of permitted uses for large, older residential structures.

Planning Board Responsibility: Primary

Time Frame: Mid Term

Status: Currently, the Planning Board is reviewing the potential to allow for additional permitted uses within the Village Commercial district in order to enable large, older residential structures to be preserved and utilized. Adaptive reuse of residential structures in other zones will be considered by the Board members as they conduct their zone-by-zone review of the Town.

Policy: Strengthen and clarify zoning provisions that allow multi-family dwellings.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Zoning Ordinance has been revised to allow for duplex units in any residential zoning district (outside of certain Watershed Protection Overlay District areas) when they are utilized as low or moderate income housing in conjunction with a land development or subdivision. Additionally, a "Low and Moderate Income Housing" section has been added to the Zoning Ordinance, outlining the requirements for Comprehensive Permit applications, which can allow for multi-family housing in any district if a permit is granted.

Policy: Evaluate zoning provisions that allow mixed use residential.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Zoning Ordinance has been amended to allow for mixed-use residential development within the Highway Commercial Zoning District. Amendments pertaining to mixed-use development within the General Commercial Zoning District have also been adopted. Mixed use development potential within the Village Commercial Zoning District is currently under review.

Policy: Continue to mandate the set aside of a minimum of 20% and 30% affordable units for single family, and multi-family developments, respectively.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Board continues to require affordable housing set asides for all Major Subdivisions and Land Developments (six or more units / lots) in accordance with the inclusionary zoning provisions of the Tiverton Zoning Ordinance.

Policy: Adopt zoning ordinance requirements to allow the development of a mixed-use town center and lifestyle center developments that require a minimum of 30% affordable housing units.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: In 2005, draft ordinances were presented by the Planning Board (IPOD – Interim Planning Overlay District) and a private developer (TCOD – Town Center Overlay District). The Town Council rejected both proposals.

Policy: Work with Church Community Housing Corporation and other entities to redevelop the Bourne Mill complex into a mixed use development that includes significant affordable housing units.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: Armory Revival Corporation and EA Fish Associates have partnered to redevelop the Bourne Mill complex. To date, one hundred and sixty-six (166) residential units have been developed. Ninety-nine (99) of these units qualify as “low and moderate income” dwelling units. The project has been approved for up to 290 total residential units, including rental and condominiums, as well as up to thirty thousand (30,000) square feet of commercial space.

Policy: Support the development of specialized residential compounds which provide affordable housing options in the more rural portions of town.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: A seventy-two (72) unit development has been approved on the Bossom Farm, located to the west of Bliss Corners on the south side of Bulgarmarsh Road (Sandywoods Farm). Fifty (50) of the available housing units are cooperative style affordable (“low and moderate income”) units. The focus of the development is arts and agriculture. The majority of the property will be preserved as open space or functional agricultural land, and commercial space will be available for a bed and breakfast, farmer’s market, artists’ sales and other arts and agricultural related businesses.

Policy: Establish an Affordable Housing Commission.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: This commission has not been established. A large number of affordable dwelling units have been created through the redevelopment of the Bourne Mill and the development of Sandywoods Farm. These two developments have created one hundred and forty nine (149) new affordable units. When coupled with smaller inclusionary zoning units, the private and non-profit sector has taken the lead in bringing Tiverton closer to the State mandated 10% affordable housing level. In recognition of the progress towards the State’s mandated goal, the Town of Tiverton was honored in April as a 2011 Partner in Housing by Rhode Island Housing and Governor Lincoln Chafee.

Policy: Amend the Zoning Ordinance so that all comprehensive permit applications are evaluated according to the goals and policies set forth in the Affordable Housing Plan.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: This policy has been implemented.

SERVICES AND FACILITIES

Policy: Develop and annually update the five year capital plan.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: This policy has not been implemented.

Policy: Implement an impact fee ordinance and consider establishing an annual cap on building permits.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: The impact fee ordinance has been implemented. While a town-wide building permit cap has not been considered, the Zoning Ordinance was amended to allow the Planning Board to cap the number of dwelling units proposed by for-profit developers and approved through the Comprehensive Permit process to an aggregate of one percent (1%) of the total number of year-round housing units in Tiverton.

Policy: Encourage the School Committee to monitor Planning Board proceedings for school enrollment potential.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: School Committee Chairwoman Sally Black attends many Planning Board meetings as a welcome member of the audience. Additionally, the School Department is invited, in writing, to comment on all Major Subdivisions and Major Land Developments as part of the review process.

Policy: Consider options for the development of a modern municipal center.

Planning Board Responsibility: Joint

Time Frame: Long Term

Status: In 2004 a Town Center study was completed by the Planning Board with assistance from consultant The Cecil Group, funding through the Community Development Block Grant (CDBG) program. One of the considerations was development of combined municipal service facilities, such as Police and Fire.

Policy: Develop a town-wide Drought Management Strategy.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: This policy was included as a requirement for State approval of the Comprehensive Community Plan. A Drought Management Strategy would be developed in conjunction with the two quasi-municipal water authorities (North Tiverton Fire District and Stone Bridge Fire District).

Policy: Provide funding to establish and implement town-wide GIS mapping.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: The Tiverton GIS is up and running, and available online at: <http://host.appgeo.com/TivertonRI/>

CIRCULATION

Policy: Apply access management tools to site plan review for commercial developments and residential subdivisions. Consider developing form-based codes.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: The Zoning Ordinance requirements for site plan review (Article XX) have been revised to create a wider scope of review for new non-residential and mixed-use development. Form-based codes are a topic for future consideration and would require a total rewrite of the existing Zoning Ordinance, which is better classified as a Long Term goal.

Policy: Utilize the Tiverton Police Department accident data to monitor areas with high accident frequency and consider design changes or improvements to mitigate and improve safety.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: Through the Federal Highway Administration's Road Safety Audit process the Town will be able to utilize accident data and other data sources to monitor problem areas and recommend changes and improvements. The Town's first Road Safety Audit was recently held for Bliss Corners. It is expected that the process will also be utilized in other areas of town.

Policy: Review all commercial development for consistency with the goal of creating a more walkable and pedestrian-friendly environment. Discourage or limit the construction of additional fast food drive-through windows.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: All developments falling within the purview of the Tiverton Planning Board are reviewed for consistency with the Comprehensive Community Plan. Fast food drive-through windows are currently only allowed in certain Zoning Districts, and only by Special Use Permit.

Policy: Preserve the public right-of-way of the Newport and Old Colony Railway and evaluate the use of the line as part of a regional transportation plan including pedestrian and bicycle access.

Planning Board Responsibility: Joint

Time Frame: Long Term

Status: The Planning Board remains cognoscente of this public right of way and its potential future uses.

Policy: Maintain and update local priorities for local pedestrian, bicycle and roadway improvements based on the Transportation Improvement Program (TIP).

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: Local priorities have been determined by the Town Council, with input from the Planning Department and Department of Public Works. This priority list has been submitted to RIDOT for inclusion on the TIP list.

OPEN SPACE, COASTAL RESOURCES AND RECREATION

Policy: Pursue the preservation of open space using selection criteria and priorities as defined.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: Preservation of open space is encouraged by the Planning Board through the use of Rural Residential Development regulations.

Policy: Require input from the Open Space Commission and the Conservation Commission in Planning Board review of proposed open space areas of RRD's. (Rural Residential Developments)

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: While the Planning Board cannot require input from various Boards and Commissions, input is always welcome. For Major Subdivisions or Land Developments (six or more dwelling units) comment is solicited in writing from many of the Town's Boards, Commissions and department heads including Conservation. For example, there is a Major Subdivision / RRD currently under review by the Planning Board. The Conservation Commission and Open Space Commission have both received plan copies, and the Open Space Commission will be given the opportunity to comment on the open space management plan.

Policy: Continue to promote the Farm, Forest and Open Space program.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board continues to promote Open Space, farm and forest conservation through the use and promotion of the Rural Residential subdivision regulations.

Policy: Support and encourage existing programs to conserve the Town's remaining farmland and actively managed forestland.

Planning Board Responsibility: Primary

Time Frame: Ongoing

Status: The Planning Board continues to promote Open Space, farm and forest conservation through the use and promotion of the Rural Residential subdivision regulations.

Policy: Preserve the public right-of-way of the Newport and Old Colony Railway for future recreational use, including pedestrian and bicycle use and waterfront access.

Planning Board Responsibility: Joint

Time Frame: Long Term

Status: See "Circulation".

Policy: Publish a town-wide map showing recreational and open space resources.

Planning Board Responsibility: Short Term

Time Frame: Short Term

Status: The Town's GIS system is available to map many town resources, such as areas zoned as Open Space and town-owned parcels. Recreational resources are also mapped within the Comprehensive Community Plan.

ECONOMIC DEVELOPMENT

Policy: Evaluate the existing and potential uses for commercially and industrially zoned land, consider options for rezoning.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: Revisions to the General Commercial Zoning District have been recommended by the Planning Board and adopted by the Town Council. The Planning Board is currently conducting a zone-by-zone review of the regulations, including all commercial and industrial districts.

Policy: Support and promote new commercial development that primarily serve local needs and reflect and/or compliment the unique character of Tiverton.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: All projects and amendments approved by the Planning Board must be in conformance with the goals and objectives of the Tiverton Comprehensive Community Plan. This policy is included in the document and is therefore considered when reviewing each new petition.

Policy: Consider the development of a “Tiverton Town Center” that consists of a comprehensive mixed use development.

Planning Board Responsibility: Primary

Time Frame: Long Term

Status: The concept of a “Tiverton Town Center” has been discussed at length by the Planning Board, resulting in the draft IPOD (Interim Planning Overlay District) Zoning Ordinance amendment, which was rejected by the Town Council in 2005. Since that time, an Informal Concept Plan was approved for mixed-use development “Tiverton Village” on Fish Road, located south of Route 24, but municipal facilities were not considered as part of this plan. The “Tiverton Village” plan has not yet returned for further review and permitting.

Policy: Adopt and apply building and site design standards for new commercial development and redevelopment.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: Zoning Ordinance Article XX – Development Plan Review, was amended in 2009, including new building and site design standards for commercial development and redevelopment. The Planning Board has appointed an ad-hoc design standards committee to further review the regulations on a zone-by-zone basis. This ad-hoc committee has held several meetings, resulting in draft Design Standards and Guidelines for the Tiverton Industrial / Enterprise Park. These Standards and Guidelines are currently under review by the full Planning Board. After the Planning Board has completed its review, the document will be forwarded to the Town Council for review and adoption.

Policy: Require fiscal impact analysis of commercial and mixed use developments.

Planning Board Responsibility: Joint

Time Frame: Ongoing

Status: Fiscal impact analyses are required for all Comprehensive Permit applications, as enabled by State Law. Impacts are also assessed for larger scale non-residential or mixed-use projects, either through the use of an independent consultant or through the solicitation of comments from town departments regarding service impacts and net tax revenue generation.

Policy: Evaluate zoning regulations addressing mixed uses.

Planning Board Responsibility: Primary

Time Frame: Short Term

Status: Mixed-use regulations have been revised to allow for the use within the Highway Commercial Zoning District (by Special Use Permit). In addition, the parking requirements for mixed-use development have been revised to allow for shared parking. Mixed use development will continue to be discussed as the Planning Board conducts its zone-by-zone review.

Policy: Adopt a long-term waterfront plan, focusing on the area north of Stone Bridge.

Planning Board Responsibility: Primary

Time Frame: Mid Term

Status: The Town Planner, as the Chairman of the Stone Bridge Committee, will be addressing this policy in conjunction with the Harbor Commission and the Town Council. In addition, the Planning Board's zoning subcommittee has started the process of a Waterfront area zoning review.

Policy: Support the artist colony and promote development of the arts.

Planning Board Responsibility: Joint

Time Frame: Short Term

Status: The "artist colony" concept has been approved through the development of the Sandywoods Farm Comprehensive Permit located west of Bliss Corners on Bulgarmarsh Road. While the term "artist colony" has been abandoned, the concept of a mixed-use development offering different housing choices along side art and agriculture-centered commercial development has been approved by the Town and is currently under construction. The Planning Board continues to promote development of the arts in other areas of Town as well.